



# City of Gainesville

Department of Sustainable  
Development

## CITY PLAN BOARD STAFF REPORT

PUBLIC HEARING DATE: February 24, 2022

PROJECT NAME AND NUMBER: Dr. Ebrahim Rezoning; PB-22-00018 ZON

APPLICATION TYPE: Quasi-Judicial

RECOMMENDATION: Approve

CITY PROJECT CONTACT: Juan Castillo

PROPERTY SIZE: 0.65 +/- Acres

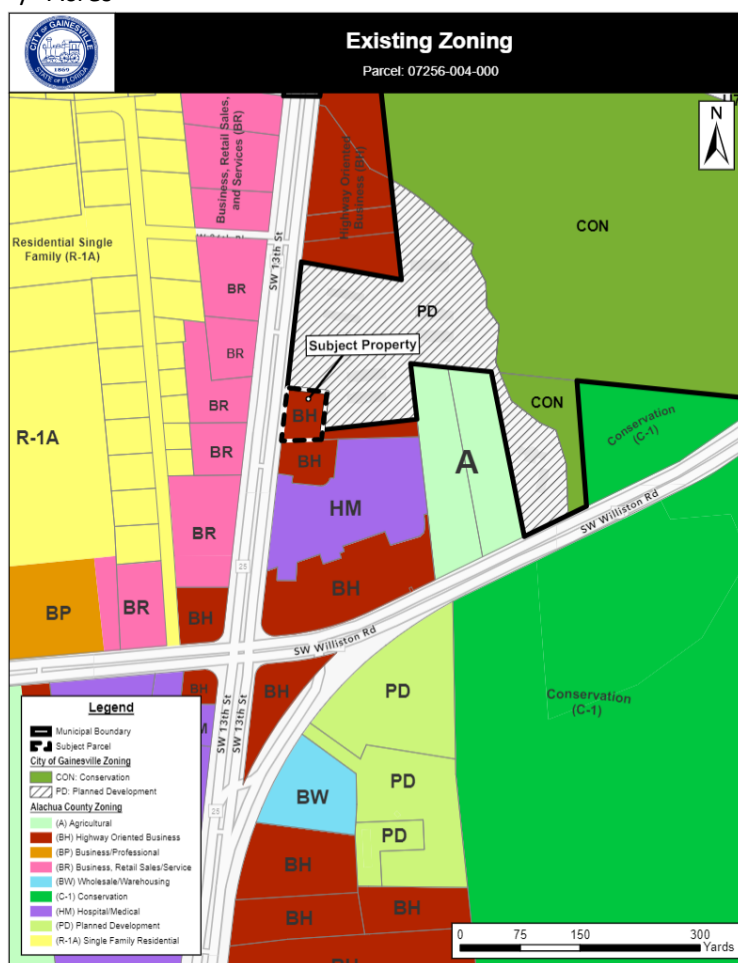
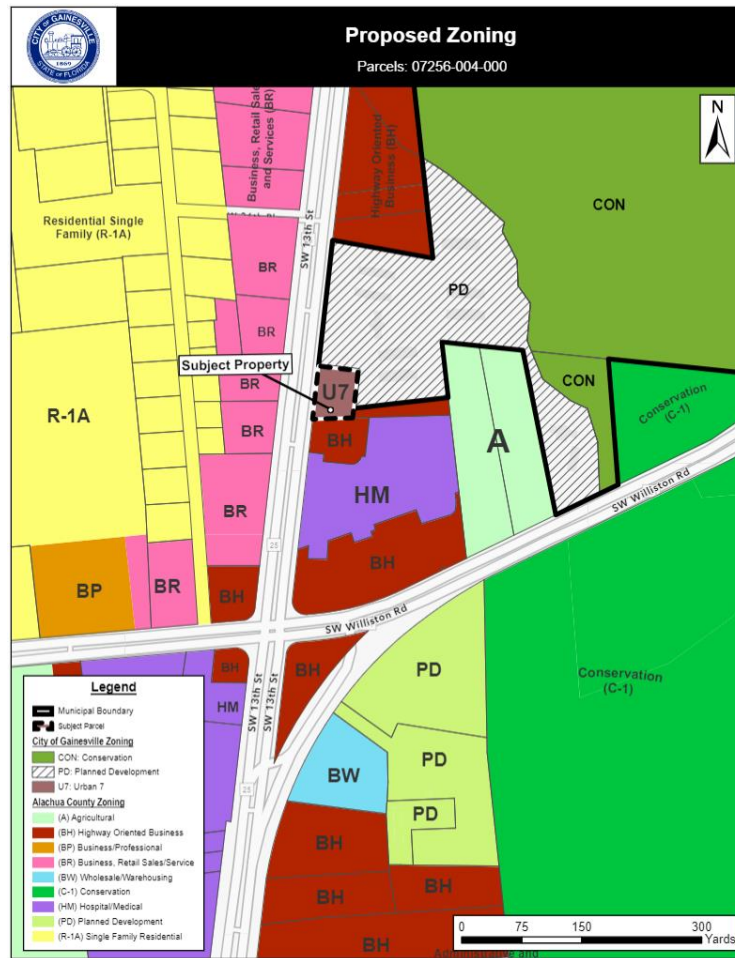


Figure 1: Existing Zoning – Alachua County Highway Oriented Business (BH)



**Figure: 2 Proposed Zoning – Transect Zoning District Urban 7 (U7)**

## **APPLICATION INFORMATION**

Agent/Applicant: City of Gainesville.

Property Owner(s): King Tut Corp. / Dr. Fawzy Ebrahim.

Related Petition(S): PB-22-00019 LUC

## **SITE INFORMATION**

Location: South of Biven's Arm and City Boundary, on SW 13<sup>th</sup> St.

Parcel Number(s): 07256-004-000

Existing Use(s): Vacant

Current Land Use Designations(s): Commercial (Alachua County)

Current Zoning Designation(s): Highway Oriented Business (BH) Alachua County

Transportation Mobility Program Area (TMPA): Zone A

**Table 1: Adjacent Property Characteristics**

	Existing Use(s)	Land Use Designation(s)	Zoning Designation(s)
North	Residential, Multi-family	Residential Low (RL) 0 to 15 DU/AC	City of Gainesville Planned Development (PD)
South	Medical/Clinical Housing	Commercial (Alachua County)	Highway Oriented Business (BH) Alachua County
East	ROW, Commercial	ROW, Commercial (Alachua County)	Retail Sales, and Services (BR) Alachua County
West	Residential, Multi-family	Commercial (Alachua County)	Planned Development (PD), Highway Oriented Business (BH) Alachua County

### Purpose and Description

This petition is a City initiated request for the rezoning of parcel 07256-004-000 from Alachua County Highway Oriented Business (HB) to urban transect zoning district U7. This request is accompanied with a request for an amendment to the Future Land Use Map via related petition PB-22-00018 LUC. This property is generally located south of the Landing at Bivens Arms development, south of the existing City boundary, on SW 13<sup>th</sup> St.

Currently, the subject parcel is vacant. The property is located in Zone A of the Transportation Mobility Program Area (TMPA) and within the UF Context Area.

The current County zoning of Highway Oriented Business allow for the development of commercial uses. The full list of allowed uses are included in Appendix D of this report.

The proposed rezoning would remove the existing Alachua County zoning district and would replace it with an urban transect zone district U7. The proposed urban transect zone U7 would increase the density of the site to 50 units per acre by right and 60 units per acre with bonus. It would also allow for the introduction of a residential component to the site. The increase in density and the expansion of uses is consistent with the existing uses in the area and the projected growth and development pattern. Furthermore, U7 zoning already exists within a quarter of a mile north of the subject parcel.

Lastly, the proposed U7 zoning district will provide the area with regulations designed to promote the creation of pedestrian friendly development by regulating sidewalk installations with buildings closer to the street and urban landscape requirements. Aside from the sidewalk requirements, transect zones also provide building design regulations designed to promote design that is scaled for pedestrians in order to promote pedestrian activity on the street.

### Provisions and Regulations

#### Recommendation Criteria

The staff recommendation is based on the five factors below:

1. Conformance with the Comprehensive Plan
2. Conformance with the Land Development Code
3. Changed Conditions
4. Compatibility
5. Impacts of Affordable Housing

## ANALYSIS

### 1. Conformance with the Comprehensive Plan

This petition is a request to rezone the subject property from Alachua County Highway Oriented Business (BH) to City of Gainesville Urban Transect Zone District U7. The rezoning is consistent with the City's Comprehensive Plan and supports the various goals of the Comprehensive Plan.

The proposed urban transect zone U7 is consistent with the proposed Urban Mixed Use (UMU) Land Use in the Related Petition PB-22-00019 LUC and has been shown to be consistent with the City's Comprehensive Plan Future Land Use element: Objective 1.5, Objective 3.4, Objective 4.1., Policy 4.2.1, and Objective 4.2.

*Objective 1.5 Discourage the proliferation of urban sprawl.*

*Objective 3.4 The City shall ensure that services and facilities needed to meet and maintain the Level of Service (LOS) standards adopted in this Plan are provided.*

*Objective 4.1 The City shall establish land use designations that allow sufficient acreage for residential, commercial, mixed use, office, industrial, education, agricultural, recreation, conservation, public facility and institutional uses at appropriate locations to meet the needs of the proposed population and that allow flexibility for the City to consider unique, innovative, and carefully construed proposals that are in keeping with the surrounding character and environmental conditions of specific sites.*

*Objective 4.2 The City shall implement regulations that will protect low-intensity uses from the negative impacts of high-intensity uses and provide for the healthy coexistence and integration of various land uses.*

*Policy 4.2.1 The City shall adopt land development regulations that provide protection for adjacent residential areas and low intensity uses from the impacts of high intensity uses by separating intense uses from low-intensity use by transitional uses and by performance measures. Performance measures shall address the buffering of adjacent uses by landscape, building type and site design. Regulation of building type shall insure compatibility of building scale, and overall building appearance in selected areas. Regulation of the site design shall address orientation. Such regulation shall also include arrangement of functions within a site, such as parking, loading, waste disposal, access*

*points, outdoor uses and mechanical equipment; and the preservation of site characteristics such as topography, natural features and free canopy.*

## **2. Conformance with the Land Development Code**

The proposed rezoning to urban transect zone U7 will complement the Urban Mixed Use (UMU) land use category proposed by related petition PB-22-00019 LUC. The review criteria for rezoning are in Land Development Code Article III, Division 3 – Text Amendments. Land Use and Zoning Changes (see Appendix B, Land Development Code).

Any proposed development or redevelopment will be required to meet all applicable Land Development Code requirements at the time of development plan review.

## **3. Changed Conditions**

Changed conditions includes a higher proposed density than what currently exists. The proposed urban transect zone district U7 would allow for 50 dwelling units per acre by right and 60 dwelling units per acre with bonus as well as introduce a residential component to the subject property. This would densify and intensify the subject parcel. The higher density and intensity of the proposal is consistent with the zoned intensity and density in the area. The proposed transect zone U7 will also introduce a more diverse list of uses for the area. Lastly, the proposed urban transect zoning district U7 will introduce transect building form, building design, and parking requirements designed to create a more urban and pedestrian friendly environment.

## **4. Compatibility**

As mentioned earlier in the report, the subject parcel is currently vacant. The proposed rezoning would enable the subject property to develop in a manner that is consistent in character with the surrounding commercial and institution development as well as introducing a residential component consistent with the multi-family development present in the area.



Subject property on the left of the image. Currently, the property is vacant.



Restaurant and motel developments in front of the subject property across SW 13<sup>th</sup> St.





**Multi-family development north of the subject property.**

Proposed development intensities for the subject property are higher than what the Alachua County zoning allows. However, the proposed intensity is consistent with the development in the area and would serve as a continuation of the higher intensity development trend. Furthermore, the proposed U7 zoning district will allow for higher densification and the incorporation of a residential element consistent with the goals and objective of the City of Gainesville Comprehensive Plan.

## **5. Impacts on Affordable Housing**

This rezoning is not expected to have a negative impact on affordable housing. However, the proposed zoning district would allow for the incorporation of a residential element at a density of 50 units per acre by right and 60 units per acre with bonus. This would provide the development opportunity for mixed use and would also allow the opportunity for diverse housing and residential options.

## **6. Impacts on Transportation**

This property is served by SW 13<sup>th</sup> St and as such no immediate changes to the transportation network is expected. The property is currently within Transportation Mobility Program Area (TMPA) zone A. Any future development on the site will be subject to the criteria as outlined within the Comprehensive Plan concerning transportation, pedestrian, and bicycle connectivity for the area. Furthermore, property is served by RTS bus route 13 and has sidewalks and bike lanes on SW 13<sup>th</sup> St.

## **7. Environmental Impacts and Constraints**

The subject parcel does not contain wetlands, FEMA Floodplain, Strategic Ecosystems or other environmental features. However, the Bivens Arm Nature Park and Sweetwater Wetlands Park are

within a quarter of a mile from the subject property. Adverse impact from the subject property unto Bivens Arm Nature Park and Sweetwater Wetlands Park is not expected.

#### **RECOMMENDATION**

Staff recommends the approval of PB-22-00018 ZON rezoning of parcel 07256-004-000 from Alachua County Business Oriented Highway (BH) to Urban Transect Zoning District U7.

#### **DRAFT MOTION FOR CONSIDERATION**

**Approve PB-22-00018 ZON** rezoning of parcel 07256-004-000 from Alachua County Business Oriented Highway (BH) to Urban Transect Zoning District U7.

#### **LIST OF APPENDICES:**

**Appendix A:** Comprehensive Plan Goals, Objectives and Policies

A-1 Comprehensive Plan Future Land Use Element

A-2 Relevant Comprehensive Plan Goals, Objectives, Policies (GOP)

A-3 Comprehensive Plan – Transportation Mobility Element

**Appendix B:** Land Development Code Regulations

**Appendix C:** Maps

**Appendix D:** Site Notification



## **APPENDIX A: Comprehensive Plan Goals, Objectives and Policies**

## **Appendix A: Comprehensive Plan Goals, Objectives and Policies**

### **A-1 Comprehensive Plan Future Land Use Element**

Policy 4.1.1 Land Use Categories on the Future Land Use Map shall be defined as follows:

**Urban Mixed-Use (UMU): up to 60 units per acre; and up to 20 additional units per acre by Special Use Permit**

This land use category allows residential, office, retail and serve uses either as stand-alone uses or combined in a mixed-use development format. Light assembly, fabrication, and processing uses within fully enclosed structures shall be allowed in specified zoning districts as specially regulated by the Land Development Code. Structures in this category shall be oriented to the street and encouraged multi-modal transportation through the development design. Developments located within this category shall be scaled to fit the character of the area. Residential density shall be limited to 60 units per acre with provisions to add up to 20 additional units per acre by Special Use permit as specified in the land development regulations. Maximum building height shall range between 4 to 5 stories, depending upon the implementing zoning district, with provisions to add up to an additional 1 to 2 stories by a height bonus system as established in the Land Development Code. Land development regulations shall set the appropriate densities, the types of uses; design criteria; landscaping, and pedestrian/vehicular access. Public and private schools, places of religious assembly and community facilities are appropriate within this category.

### **A-2 Relevant Comprehensive Plan Goals, Objectives, Policies (GOP)**

**Objective 1.5      Discourage the proliferation of urban sprawl.**

**Objective 3.4      The City shall ensure that services and facilities needed to meet and maintain the Level of Service (LOS) standards adopted in this Plan are provided.**

**Objective 4.1** The City shall establish land use categories that allow sufficient acreage for residential, commercial, mixed-use, office, industrial, education, agricultural, recreation, conservation, public facility, and institutional uses at appropriate locations to meet the needs of the projected population and that allow flexibility for the City to consider unique, innovative, and carefully construed proposals that are in keeping with the surrounding character and environmental conditions of specific sites. Land use categories associated with transect zones are intended to encourage a more efficient and sustainable urban form by allowing a range of housing, employment, shopping and recreation choices and opportunities in a compact area of the City.

**Objective 4.2** The City shall implement regulations that will protect low-intensity uses from the negative impacts of high-intensity uses and provide for the healthy coexistence and integration of various land uses.

**Policy 4.2.1** The City shall adopt land development regulations that provide protection for adjacent residential areas and low intensity uses from the impacts of high intensity uses by separating intense uses from low-intensity uses by transitional uses and by performance measures. Performance measures shall address the buffering of adjacent uses by landscape, building type and site design. Regulation of building type shall insure compatibility of building scale, and overall building appearance in selected areas. Regulation of site design shall address orientation. Such regulation shall also include arrangement of functions within a site, such as parking, loading, waste disposal, access points, outdoor uses and mechanical equipment; and the preservation of site characteristics such as topography, natural features and tree canopy.

### **A-3 Comprehensive Plan – Transportation Mobility Element**

**Objective 10.1** The Gainesville Transportation Mobility Program Area (TMPA) shall include all property within city limits (although the TMPA shall not apply to annexed properties that do not yet have an adopted City land use category) and shall be subdivided into designated Zones A, B, C, D, E and M as mapped in the Transportation Mobility Element Data and Analysis Report and in the Geographic Information System (GIS) Map Library located on the City's Planning and Development Services Department website.

- Policy 10.1.1 All property within city limits is included in the Gainesville Transportation Mobility Program Area (TMPA); however, the TMPA shall not apply to annexed properties that do not yet have an adopted City land use category. When annexed properties are designated with a City land use category, they shall be assigned to the most physically proximate TMPA zone as mapped in the Transportation Mobility Element Data and Analysis Report and in the GIS Map Library on the City's Planning and Development Services Department website.
- Policy 10.1.2 All land uses and development located in the TMPA shall meet the TMPA policies specified in this Element.

- Policy 10.1.3      Zone A shall promote redevelopment and infill in the eastern portion of the City and the area near the University of Florida. Except as shown in Policy 10.1.4 and Policy 10.1.14, funding for multi-modal transportation in Zone A shall be provided to the maximum extent feasible by the City, Community Redevelopment Agency, federal or state governments, and other outside sources such as grant funds.
- Policy 10.1.4      For any development or redevelopment within Zone A, the developer shall provide the following transportation mobility requirements. The developer shall provide any transportation modifications that are site related and required for operational or safety reasons, such as, but not limited to, new turn lanes into the development, driveway modifications, or new traffic signals, and such operational and safety modifications shall be unrelated to the Transportation Mobility Program requirements.
- a.      Sidewalk connections from the development to existing and planned public sidewalk along the development frontage;
  - b.      Cross-access connections/easements or joint driveways, where available and economically feasible;
  - c.      Deeding of land or conveyance of required easements along the property frontage to the City, as needed, for the construction of public sidewalks, bus turn-out facilities, and/or transit shelters. Such deeding or conveyance of required easements, or a portion of same, shall not be required if it would render the property unusable for development. A Transit Facility License Agreement between the property owner and the City for the placement of a bus shelter and related facilities on private property may be used in lieu of deeding of land or conveyance of easements. The License Agreement term shall be for a minimum of 10 years;
  - d.      Closure of existing excessive, duplicative, or unsafe curb cuts or narrowing of overly wide curb cuts at the development site, as defined



in the Access Management portion of the Land Development Code;  
and

- e. Safe and convenient on-site pedestrian circulation, such as sidewalks and crosswalks connecting buildings and parking areas at the development site.

**Policy 10.1.5** For any development or redevelopment within Zones B, C, D, E, or M, the developer shall provide all of the items listed in Policy 10.1.4 and shall provide the transportation mobility requirements as specified in Policies 10.1.6, 10.1.7, 10.1.9, 10.1.11, 10.1.13, and 10.1.14, as applicable. The developer shall also provide any transportation modifications that are site related and required for operational or safety reasons, such as, but not limited to, new turn lanes into the development, driveway modifications, or new traffic signals, and such operational and safety modifications shall be unrelated to the Transportation Mobility Program requirements.

**Policy 10.1.6** For any development or redevelopment within Zone B, the developer shall, at the developer's expense, meet the following transportation mobility criteria based on the development's (including all phases) trip generation and proportional impact on transportation mobility needs. The criteria chosen shall relate to the particular development site and the transportation mobility conditions and priorities in the zone, adjacent zones, and/or citywide for criteria that benefit the overall transportation system. Based on cost estimates provided by the developer and verified by the City, the City shall have the discretion to count individual criteria as equivalent to two or more criteria for purposes of satisfying transportation mobility requirements. Provision of the required transportation mobility criteria shall be subject to final approval by the City during the development review process and shall be memorialized in a TMPA agreement between the City and the developer.

<b>Net, New Average Daily Trip Generation</b>	<b>Number of Criteria That Shall Be Met</b>
50 or less	At least 1
51 to 100	At least 2
101 to 400	At least 3
401 to 1,000	At least 5
1,001 to 5,000	At least 8

Greater than 5,000	<p>At least 12 and meet either a. or b.:</p> <ul style="list-style-type: none"> <li>a. Located on an existing RTS transit route with minimum 15-minute frequencies in the a.m. and p.m. peak hours.</li> <li>b. Provide funding for a new RTS transit route with minimum 15-minute frequencies in the a.m. and p.m. peak hours or provide funding to improve RTS transit headways to minimum 15-minute frequencies in the a.m. and p.m. peak hours. Funding for new routes shall include capital and operating costs for a minimum of 5 years. Funding for existing route expansions or enhancements shall include capital and operating costs for a minimum of 3 years.</li> </ul>
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Policy 10.6.1      Parking in excess of that required by the Land Development Code shall be prohibited within the TMPA.

Policy 10.6.2      Developments may apply for a parking reduction within the TMPA, based on criteria in the Land Development Code.

## **APPENDIX B: Land Development Code Regulations**

## Appendix B – Land Development Code Regulations

## B-1 Governing Principles

- 16 **Section 30-3.14. Rezoning Criteria.**
- 17 Applications to rezone property shall be reviewed according to the following criteria:
- 18 A. Compatibility of permitted uses and allowed intensity and density with surrounding existing  
19 development.
- 20 B. The character of the district and its suitability for particular uses.
- 21 C. The proposed zoning district of the property in relation to surrounding properties and other similar  
22 properties.
- 23 D. Conservation of the value of buildings and encouraging the most appropriate use of land throughout  
24 the city.
- 25 E. The applicable portions of any current city plans and programs such as land use, traffic ways,  
26 recreation, schools, neighborhoods, stormwater management and housing.
- 27 F. The needs of the city for land areas for specific purposes to serve population and economic  
28 activities.
- 29 G. Whether there have been substantial changes in the character or development of areas in or near  
30 an area under consideration for rezoning.
- 31 H. The goals, objectives, and policies of the Comprehensive Plan.
- 32 I. The facts, testimony, and reports presented at public hearings.
- 33 J. Applications to rezone to a transect zone shall meet the following additional criteria:
- 34 1. The proposed T-Zone shall provide a logical extension of an existing zone, or an adequate  
35 transition between zones.
- 36 2. The area shall have had a change in growth and development pattern to warrant the rezoning to  
37 a more or less urban T-Zone.

- 1 3. The request shall be consistent with the overall City of Gainesville vision for growth and  
2 development as expressed in the City of Gainesville Comprehensive Plan.
- 3 4. If not adjacent to an existing T-Zone, the rezoning site shall comprise a minimum of 10 acres.

## B-2 Corresponding with Future Land Use Categories.

2 **Section 30-4.2. Correspondence with Future Land Use Categories.**

3 The following table establishes the zoning districts allowable within the future land use categories from  
 4 the Comprehensive Plan.

Future Land Use Category	Zoning Districts	Special Districts
Single-Family (SF)	U1, RSF-1 to 4, RSF-R	PD, CON, PS
Residential Low-Density (RL)	U2, RSF-4, RMF-5, MH, RC	PD, CON, PS
Residential Medium-Density (RM)	RMF-6 to 8	PD, CON, PS
Residential High-Density (RH)	N/A	PD, CON, PS
Mixed-Use Residential (MUR)	U5	PD, CON, PS
Mixed-Use Office/Residential (MOR)	U4, U3	PD, CON, PS
Mixed-Use Low-Intensity (MUL)	MU-1	PD, CON, PS
Mixed-Use Medium-Intensity (MUM)	MU-2, CP	PD, CON, PS
Urban Mixed-Use (UMU)	U6, U7, U8	PD, CON, PS
Urban Mixed-Use High-Intensity (UMUH)	U9	PD, CON, PS
Urban Core (UC)	DT	PD, CON, PS
Office (O)	OR, OF, CP	PD, CON, PS, MD
Commercial (C)	W, BA, BT, BUS, BI	PD, CON, PS
Business Industrial (BI)	BI, CP	PD, CON, PS
Industrial (IND)	W, I-1, I-2, BI	PD, CON, PS
Education (E)	N/A	PD, CON, PS, ED
Recreation (REC)	N/A	PD, CON, PS
Conservation (CON)	N/A	PD, CON, PS
Agriculture (AGR)	N/A	PD, CON, PS, AGR
Public and Institutional Facilities (PF)	N/A	PD, CON, PS, AF
Planned Use District (PUD)	N/A	PD

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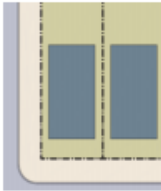
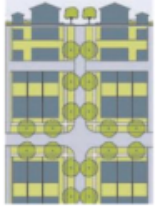
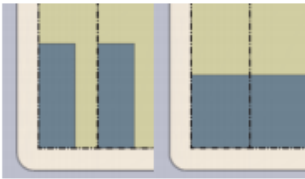
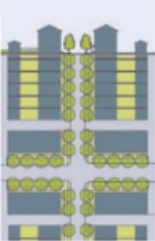
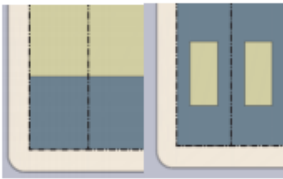
B-3. Transects

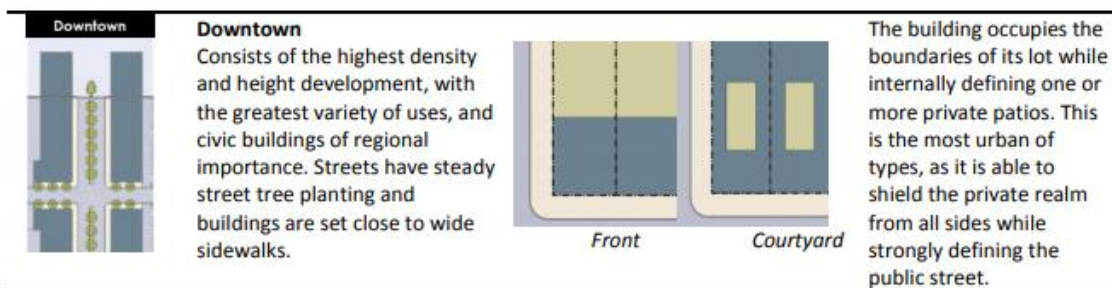


## DIVISION 2. TRANSECTS

### Section 30-4.11. Generally.

- A. *Intent*. The intent of this division is to establish development standards that will encourage a more efficient and sustainable urban form by allowing a range of housing, employment, shopping and recreation choices and opportunities in a compact, pedestrian-friendly environment.
- B. *Transects*. A transect is a geographical cross-section of a region that reveals a sequence of environments that ranges from rural to urban. Using the transect to regulate development ensures that a community offers a full diversity of development types, and that each has appropriate characteristics for its location. There are typically six transects organizing the components of place-making: Natural Zone, Rural Zone, Urban Neighborhood Zone, General Urban Zone, Urban Center Zone, and Urban Core. This code assigns transects that are tailored to the unique character of the City of Gainesville. The allowable uses, dimensional standards, and development requirements for these zones are described within this division.

	ZONE DESCRIPTION	INTENDED SITE LAYOUT
 <p><b>Urban Zone 1</b></p>	<p><b>Urban Zone 1</b></p> <p>Consists of predominantly low to medium density residential areas, adjacent to higher intensity zones where neighborhood services and mixed use are permitted.</p>	 <p><i>Edge</i></p> <p>Buildings typically occupy the center of the lot with setbacks on all sides. The front yard is intended to be visually continuous with the yards of adjacent buildings.</p>
 <p><b>Urban Zones 2 - 5</b></p>	<p><b>Urban Zones 2 - 5</b></p> <p>Consists of a wide range of residential building types. Higher number zones provide for the integration of offices and neighborhood services within an increasingly urban fabric. Setbacks and landscaping are variable. Streets with curbs and sidewalks define medium-sized blocks.</p>	 <p><i>Side</i>      <i>Front</i></p> <p>Buildings typically occupy one side of the lot leaving a setback to the other side to allow for access or privacy. In many instances, the building occupies the entire lot frontage. A shallow frontage setback defines a more urban condition.</p>
 <p><b>Urban Zones 6 - 9</b></p>	<p><b>Urban Zones 6 - 9</b></p> <p>Consists of higher density mixed use buildings that accommodate retail, offices, and apartments. It has a tight network of streets, with wide sidewalks, steady street tree planting and buildings set close to the sidewalks.</p>	 <p><i>Front</i>      <i>Courtyard</i></p> <p>The building typically occupies the full frontage, leaving the rear of the lot as the sole yard. The continuous facade steadily defines the public street. In its residential form, this type is the attached dwelling. The rear yard can accommodate substantial parking.</p>



- 1  
2 C. *Streets*. Within the transects, new developments shall connect to the existing street network.  
3 Where the existing street network is deficient with respect to gridded connectivity, the  
4 development shall provide new street connections to meet the block perimeter requirements.
- 5 The zoning map identifies a hierarchy of street types that determine the relationship of buildings to  
6 the street and the standards for the design of street landscaping and sidewalks. Street types  
7 include: Storefront, Principal, Thoroughfare, and Local Streets. All undesignated existing streets are  
8 assumed to be Local Streets. Below is a detailed description of the general function, character, and  
9 elements of each street type. Final street design for new development shall be in accordance with  
10 this article and the Design Manual, subject to review and approval by the City Manager or designee  
11 in the development review process. In the event of a conflict between the standards in this article  
12 and the Design Manual, the Design Manual shall govern.
- 13 a. *Storefront*. Storefront streets are designed to encourage a high level of pedestrian activity.  
14 Higher intensity and density uses front this street type. Due to the level of pedestrian activity  
15 on this street type, minimum sidewalk widths are increased and first floor residential  
16 development requires a 1.5 foot elevation above grade (3 feet recommended) for the privacy  
17 and comfort of residents. Building front entrances shall be oriented to this street type when  
18 there are multiple street frontages for the property.



- 19  
20 b. *Principal*. Principal streets include lower levels of pedestrian activity compared to storefront  
21 streets. This street type is located in mixed-use areas where the traffic volume is anticipated to  
22 be higher than on local streets. This street type allows for some auto-oriented uses with a  
23 special use permit.



- c. *Thoroughfares*. Thoroughfares are streets that carry high volumes of through traffic at higher speeds with less pedestrian activity and higher vehicular speeds than other street types. These streets are often fronted by larger scale commercial development and are usually 4-lane or 6-lane streets.

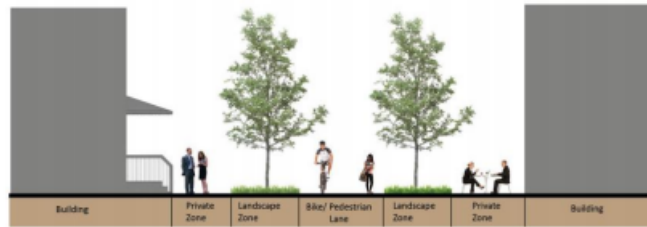


- d. *Local*. Local Streets are intended to have slow speeds and provide for connections within neighborhoods and between residential areas and commercial areas. Local Streets are not identified on the zoning map, but make up the vast majority of the street types within the transects. Any street that is not identified as a Storefront, Principal, or Thoroughfare street should be assumed to be a Local Street.



- e. *Urban Walkways*. Urban Walkways are pedestrian/bicycle pathways that serve to improve pedestrian/bicycle connectivity, delineate blocks and provide for expanded pedestrian space. These may be established as alternatives to new streets in some locations.





- f. *Alleys.* Alleys are narrower streets that are primarily used for service access to developments, or vehicular access to rear parking areas. Alleys are encouraged to be preserved, improved, or established in conjunction with development.



#### Section 30-4.12. Permitted Uses.

The following table contains the list of uses allowed, and specifies whether the uses are allowed by right (P), accessory to a principal use (A), or by special use permit approval (S). Blank cells indicate that the use is not allowed. No variances from the requirements of this section shall be allowed.

**Table V - 1: Permitted Uses within Transects.**

	Use Standards	U1	U2	U3	U4	U5	U6	U7	U8	U9	DT
<b>RESIDENTIAL</b>											
Single-family dwellings		P	P	P	P	P	P	P	P	P	P
Attached dwellings (up to 6 attached units)		-	P	P	P	P	P	P	P	P	P
Multi-family, small-scale (2-4 units per building)		-	P	P	P	P	P	P	P	P	P
Multi-family dwellings		-	-	P	P	P	P	P	P	P	P
Accessory dwelling units	30-5.33	-	P	P	P	P	P	P	P	-	-
Adult day care homes	30-5.2	P	P	P	P	P	P	P	P	P	P
Community residential homes (up to 6 residents)	30-5.6	P	P	P	P	P	P	P	P	-	-
Community residential homes (more than 6 residents)	30-5.6	-	-	P	P	P	P	P	P	P	-
Dormitory (small)	30-5.8	-	P	P	P	P	P	P	P	P	P
Dormitory (large)	30-5.8	-	-	P	P	P	P	P	P	P	P

[illegible]



	Use Standards	U1	U2	U3	U4	U5	U6	U7	U8	U9	DT
use)											
Parking, structured (principal use)		-	-	-	-	-	-	P	P	P	P
Passenger transit station		-	-	-	-	-	-	-	P	P	P
Personal services		-	-	-	P	-	P	P	P	P	P
Places of religious assembly	30-5.21	S	P	P	P	P	P	P	P	P	P
Professional school		-	-	-	P	P	P	P	P	P	P
Public administration buildings		-	-	-	S	S	S	P	P	P	P
Public parks		P	P	P	P	P	P	P	P	P	P
Recreation, indoor <sup>2</sup>		-	-	-	-	-	P	P	P	P	P
Recreation, outdoor		-	-	-	-	-	-	P	P	P	P
Research development & testing facilities		-	-	-	-	-	-	P	P	P	P
Residences for destitute people	30-5.22	-	-	-	-	-	-	-	S	S	S
Restaurant		-	-	-	S	-	P	P	P	P	P
Retail sales		-	-	-	-	-	P	P	P	P	P
School, elementary, middle & high (public & private)		S	S	S	P	P	P	P	P	P	P
Scooter and electric golf cart sales		-	-	-	-	-	-	P	P	P	-
Simulated gambling establishments		-	-	-	-	-	-	-	-	-	-
Social service facilities	30-5.25	-	-	-	-	-	-	-	P	P	P
Skilled nursing facility		-	-	-	P	-	P	P	P	P	P
Vehicle sales and rental (no outdoor display)		-	-	-	-	-	-	P	P	P	P
Vehicle services	30-5.28	-	-	-	-	-	-	P	P	-	-
Vehicle repair	30-5.28	-	-	-	-	-	-	P	-	-	-
Veterinary services	30-5.29	-	-	-	P	-	P	P	P	P	P
Vocational/Trade school		-	-	-	-	-	S	P	P	P	P
Wireless communication services	See 30-5.30										

1 **LEGEND:**

2 P = Permitted by right; S = Special Use Permit; A = Accessory; Blank = Use not allowed.

3 1 = When located along a Principal Street.

4 2 = Prohibited where adjacent to single-family zoned property.

5 3 = Office uses as a home occupation.

6 4 = Office uses up to 20% of the building square footage and shall be secondary to a principal residential

7 use. No outdoor storage allowed.

### Section 30-4.13. Building Form Standards.

This section contains the building form standards that determine the location, scale and massing of all buildings within the transects.

**Table V - 2: Building Form Standards within Transects.**

TRANSECT	U1	U2	U3	U4	U5	U6	U7	U8	U9	DT
<b>A. BLOCK STANDARDS</b>										
Block perimeter (max feet)	2,600'								2,000'	1,600'
<b>B. LOT CONFIGURATION</b>										
Lot width (min feet)	34'	18'							18'	18'
<b>C. DEVELOPMENT INTENSITY</b>										
Nonresidential building coverage (max)	60%	80%							90%	100%
Residential density by right/with bonus <sup>2</sup> (max units per acre)	8	15	20	20	75	50/60	50/60	60/80	100/125	150/175
<b>D. BUILDING FRONTAGE</b>										
Primary frontage (min)	50%	60%							70%	80%
Secondary frontage (min)	30%	40%							50%	60%
<b>E. BUILDING PLACEMENT</b>										
min-max from curb										
min landscape/min sidewalk/min building frontage										
Storefront Street	20'-25' 5'/10'/5'	20'-25' 5'/10'/5'							20'-25' 5'/10'/5'	20'-25' 4'/10'/5'
Principal Street	17'-37' 6'/6'/5'	17'-27' 6'/6'/5'							17'-27' 6'/6'/5'	17'-27' 6'/6'/5'
Thoroughfare Street	19'-100' 6'/6'/5'	19'-100' 8'/6'/5'							19'-100' 8'/6'/5'	19'-100' 8'/6'/5'
Local Street	15'-35' 5'/5'/5'	15'-20' 5'/5'/5'							16'-21' 5'/6'/5'	15'-20' 4'/6'/5'
<b>F. BUILDING SETBACKS</b>										
Side interior setback (min)	5'	5'	5'	5'	5'	5'	5'	0'	0'	0'

Rear setback (min)	15'	3' (alley) 10' (no alley)	3' (alley) 5' (no alley)	3' (alley) 0' (no alley)
--------------------	-----	------------------------------	-----------------------------	-----------------------------

1 **LEGEND:**

2 1 = See Section 30-4.8 for development compatibility standards.

3

[illegible]

### G. BUILDING HEIGHT

Min feet	NA	NA	NA	NA	NA	NA	18	18	18	18
Max stories (by right <sup>1</sup> /with bonus <sup>2</sup> )	3	3	3	3	4	4/5	4/6	5/6	6/8	12/14
Max feet (by right/with bonus <sup>2</sup> )	36	36	36	42	60	60/74	60/88	74/88	88/116	172/200

### H. FLOOR HEIGHT

Min first floor height (residential / nonresidential)	NA/10'	NA/12'	NA/12'	NA/12'	NA/12'	NA/12'	12'/12'	12'/15'	12'/15'	12'/15'
---	--------	--------	--------	--------	--------	--------	---------	---------	---------	---------

## I. GLAZING

Min first floor - nonresidential	-	30%	50%	65%
Min first floor - multi-family	-	30%		

Min upper floors - nonresidential and multi-family	-	15%
--	---	-----

- 1 **LEGEND:**  
2 1 = See development compatibility standards in Section 30-4.8.  
3 2 = See bonus system requirements in Section 30-4.9.

A. *Block standards.*

1. *Maximum block perimeter.* Maximum block perimeters are defined Table V-2 for each transect. When development cumulatively includes 50% or more of the total project area, it shall be required to include new local streets or urban walkways and the resulting block(s) shall not exceed the prescribed maximum block perimeter. Figure V-1 below depicts a recommended approach to breaking down large blocks to provide a new street grid on a large site.

**Figure V - 1: Creating Blocks**



Step 1-original site; Step 2-introduce streets; Step 3-introduce alleys; Step 4-introduce lots.

2. *Construction of new streets.*

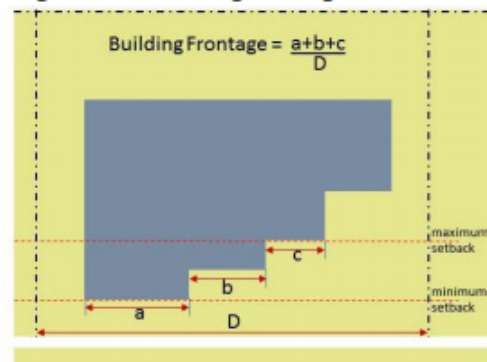
- a. The required local streets or urban walkways shall be constructed at the expense of the owner/developer as part of the development review process and shall be constructed according to the appropriate city standards, but may be sited and configured in a manner so that they provide the most appropriate access to the development. Where a street is planned to continue beyond the extent of a development, the development shall provide for the continuation of the street by stubbing out the improvements as close as is practicable to edge of the property boundary.
- b. The required local streets, multi-use paths or urban walkways shall provide for public access and may be dedicated for public right-of-way after construction, if the city desires to accept same for maintenance.
- c. Notwithstanding any other provision in this chapter, a development may receive final approval prior to construction of the required local streets or urban walkways if the city, upon approval of the City Commission, has executed a binding agreement with the owner/developer that:
  - i. Requires the city and/or the Community Redevelopment Agency to construct the required local streets as public streets within two years of final approval; and
  - ii. Provides for the conveyance or dedication of the associated right-of-way from the property owner to the city, at no cost to the city.

The city may enter into such an agreement only when the city determines that doing so would be in the public interest and when the city and/or the Community Redevelopment Agency has budgeted legally available funds for the construction of the required local



- streets. The form and content of the agreement shall be provided by and acceptable to the city in its sole discretion.
- d. Board modifications from the requirement to construct new streets may be granted in accordance with the procedures and criteria for a variance, with specific consideration given to situations where the construction of a street is limited by: access management standards, regulated environmental features, regulated natural or archeological resources, public stormwater facilities, existing utility facilities, contamination sites, inconsistencies with plans for a future city street network, parks, or schools. Where a variance from these requirements is approved, the block perimeter shall be completed with the provision of sidewalk and bicycle connections, and multi-use paths or urban walkways, subject to approval by the city.
3. *Urban walkways.* When required new streets or urban walkways are constructed as part of a subdivision or development, their design and construction shall conform to the following standards and applicable design manual standards:
- New streets or urban walkways shall connect to existing streets on abutting properties, or be constructed in alignment with planned public streets on abutting properties.
  - Where a portion of a new street or urban walkway is newly constructed, it shall be designed to be extended to abutting property. Stub-outs shall extend to the property line.
  - Urban walkways shall be a minimum of 26 feet wide and may be designed with a single or divided paved pathway. The pathway(s) shall be at least 10 feet wide in total width and shall provide for both bicycles and pedestrians. An urban walkway shall be landscaped with shade trees on minimum 50-foot centers on both sides of the paved path. Unpaved areas may also contain stormwater facilities. Urban walkways may contain benches, fountains, outdoor cafes or other outdoor uses as long as a minimum sidewalk width as specified above is maintained.
- B. *Building frontage.* Building frontage requirements shall create a continuous building presence along streets.
- The building frontage standards are a proportion of the building length relative to the width of the development site measured at the site frontage line, (see Figure V - 3).
  - Frontage hierarchy.*
    - Where a development has frontage along multiple street types that do not include a thoroughfare, the urban street (Storefront or Principal, in that order of hierarchy) shall be considered the primary street for the front face of the building.
    - Where a development has frontage on a thoroughfare and any other street type, the thoroughfare shall be considered the primary street.

**Figure V - 3: Building Frontage**



- 1 c. Where a development has frontage on two streets of equal type, then the City Manager or  
 2 designee shall make a determination as to which street frontage shall be considered  
 3 primary.

- 4 3. In the case where the required building frontage cannot be met  
 5 due to the need to provide vehicular access from the primary  
 6 frontage, a gateway, arch or similar feature may be provided to  
 7 preserve the block continuity and may be counted toward  
 8 meeting the building frontage requirement, (see Figure V - 4).

**Figure V - 4: Example of Gateway**



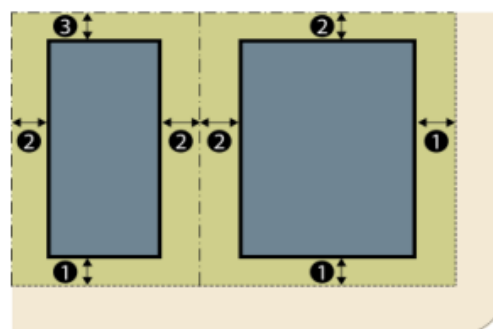
*Floor above gateway not required*

- 9 4. A preserved high quality heritage tree canopy within the street  
 10 setback range may count towards meeting the building frontage  
 11 requirement.
- 12 5. The ground floor along the street frontages shall contain active  
 13 uses oriented to the street. Active uses may include, but are not  
 14 limited to, display or floor areas for retail uses, waiting and  
 15 seating areas for restaurants, atriums or lobbies for offices, lobbies or dining areas for hotels or  
 16 multi-family residential buildings, and hotel rooms or multi-family residential units with street  
 17 facing entrances.

- 18 C. **Building placement and setbacks.** The placement of a building on a site is critical to creating a vital  
 19 and coherent public realm. The building placement and setback standards shall shape the public  
 20 realm and strengthen the physical and functional character of the area. Figure V-5 depicts the types  
 21 of setbacks.

- 22 1. Building placement requirements shall be  
 23 measured from the back of curb instead of the  
 24 front property line, with the following  
 25 exceptions:

**Figure V - 5: Building Setbacks**

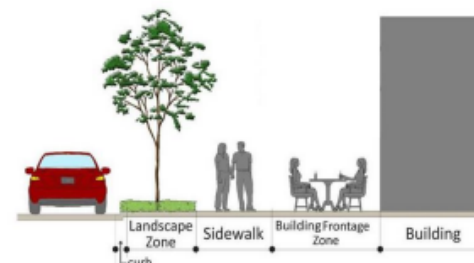


- ① Street Setback  
 ② Side Setback  
 ③ Rear Setback

- 26 a. In the absence of curbs, shall be measured  
 27 from the edge of pavement.
- 28 b. Where the required building placement falls  
 29 within a public right-of-way, it shall be  
 30 shifted to the property line instead.

- 31 2. Building placement requirements shall be  
 32 comprised of a landscape zone, a public  
 33 sidewalk zone and a building frontage zone.  
 34 Figure V-6 depicts the required configuration of these zones in relation  
 35 to the street curb and building. The required minimum widths for the  
 36 landscape and sidewalk zones are listed within Table V - 2. The building  
 37 frontage zone shall be a minimum of 5 feet in all  
 38 locations. Section 30-4.13 D contains additional  
 39 standards for the design of the building  
 40 frontage zone.

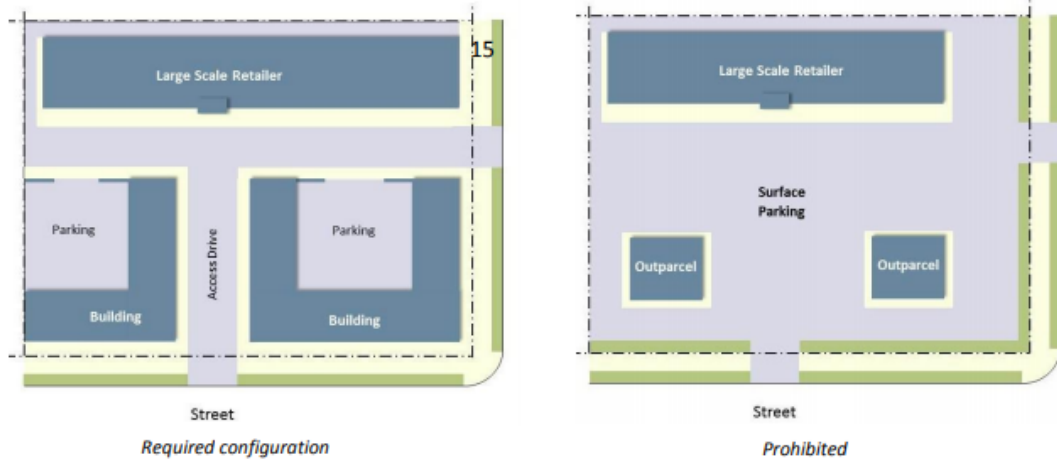
**Figure V - 6: Public Realm Zones**



- 41 3. Side and rear setbacks are minimums and shall  
 42 be measured from shared property lines.

4. The following shall not be located within the public sidewalk zone: utility poles including electrical transmission and distribution poles; light poles; mechanical equipment as defined in Section 30-6.10; signs included in Section 30-9.2A; and street furniture including benches, trash receptacles, and bicycle racks.
5. Where multiple buildings are proposed within a development, the placement of buildings at the rear of a site is allowed as long as one or more buildings are placed along the front of the site meeting the building placement and setback and building frontage requirements of this division. Figure V-7 depicts the required configuration of multiple buildings on a site, such as within a shopping center. Streets or access drives shall be incorporated into the site to break it down into smaller lots/blocks (platting will not be required). The primary access drive shall be centered on the anchor building and shall be lined with buildings, which shall meet the required frontage standards along the street and access drive.

**Figure V - 7: Multiple Buildings on a Site**



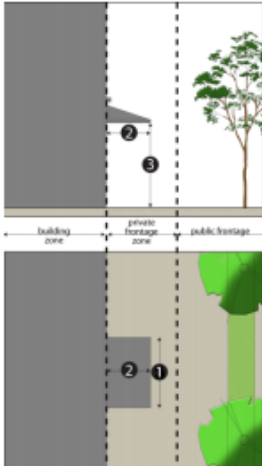
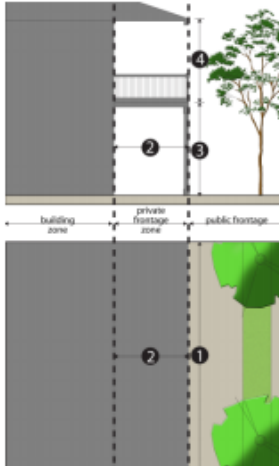
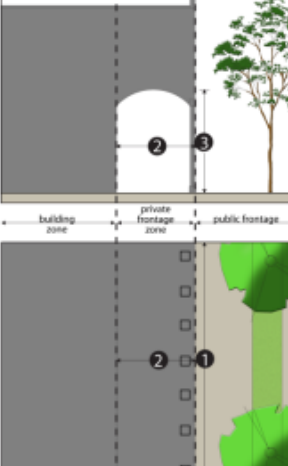
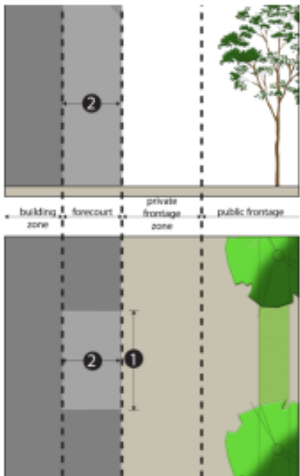
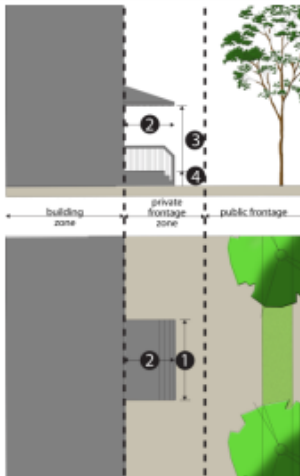
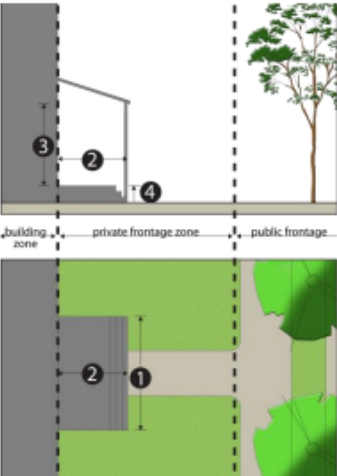


- D. *Building frontage zone requirements.* All development shall provide a minimum 5-foot wide building frontage zone behind the public sidewalk, and buildings shall have at least one type of building frontage incorporated into its design. Table V-3 contains the dimensional requirements for the various types of building frontages allowed. The intent of the building frontage zone is to provide a transition between the public street/sidewalk and the building. The type of activity conducted in the private frontage zone depends on the nature of the proposed use (Figure V-8). For a commercial building, the intent of the private frontage zone is to attract customers into the business. For a residential site, the intent of the private frontage zone is to provide for a private outdoor space and establish a separation from the public sidewalk for the ground floor rooms.

**Figure V - 8: Examples of Building Frontage Zone Activity**



1 Table V - 3: Building Frontage Dimensional Standards

Storefront	Gallery	Arcade
		
<p>1. Width: 25% of façade width min. 2. Depth: 5' min. 3. Clear Height: 8' min.</p>	<p>1. Width: 75% of façade width min. 2. Depth: 8' min. 3. Clear Height: 12' min. (1st floor)</p>	<p>1. Width: 75% of façade width min. 2. Depth: 8' min. 3. Clear Height: 12' min. (1st floor)</p>
Courtyard	Stoop	Porch
		
<p>1. Width: 10' min to 50% of façade width max. 2. Depth: 10' min/20' max. 3. Elevation: 18" max above grade.</p>	<p>1. Width: 5' min to 16' max. 2. Depth: 5' to 8' 3. Clear Height: 8' 4. Elevation: 21" min above grade.</p>	<p>1. Width: 12' min. 2. Depth: 8' min. 3. Clear Height: 8' min. 4. Elevation: 21" min above grade.</p>

2 Note: See Article II for definitions of frontages.

3 1. Building frontage standards, general.

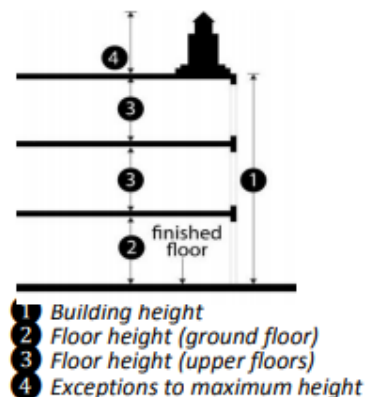




- a. In addition to the encroachments shown in Table V-3, cantilevered balconies, bay windows and roof overhangs are allowed to encroach into the building frontage zone.
  - b. Street furniture such as benches, trash receptacles, or bicycle racks may be installed within the building frontage zone; however, permanent fencing is prohibited.
2. *Standards for storefronts, awnings and canopies.*
- a. Storefront doors shall not be recessed more than 5 feet from the front façade. Recessed doors shall have angled walls leading to the door to promote the visibility of the entrance.
  - b. Awnings and canopies shall not cover building architectural elements including but not limited to cornices or ornamental features.
  - c. High gloss or plasticized fabrics and aluminum are not allowed for awnings.
  - d. Backlit awnings are not permitted.
  - e. Awning shall match the width of the window or door opening and shall enhance the architectural features of the building.
3. *Standards for galleries and arcades.*
- a. Along urban Storefront streets, gallery/arcade openings shall align with storefront entrances.
  - b. Galleries may be one or two stories.
  - c. Arcades and galleries shall have consistent depth along a frontage.
4. *Standards for courtyards.*
- a. Courtyards shall be paved and a minimum of 20% of the total courtyard area shall be enhanced with either above-ground or in-ground landscaping.
5. *Standards for stoops and porches.*
- a. Stoops shall align directly with the building entry.
  - b. Porches may be one or two stories.
  - c. Porches may encroach into the building frontage zone.
- E. *Building height.*

- 1. The heights of parking structures shall be limited in accordance with the maximum feet within each district, but shall not be limited to the maximum number of stories.
- 2. Mezzanines that meet the definition and requirements of the Florida Building Code shall not be counted as an additional story.
- 3. The building height limitations contained in Table V - 2 do not apply to spires, belfries, cupolas, antennas, water tanks, ventilators, chimneys or other appurtenances required to be placed on the roof and not intended for human occupancy. Other exceptions include:
  - a. Roof structures above eave line can vary in height up to a

**Figure V - 9: Building Height**

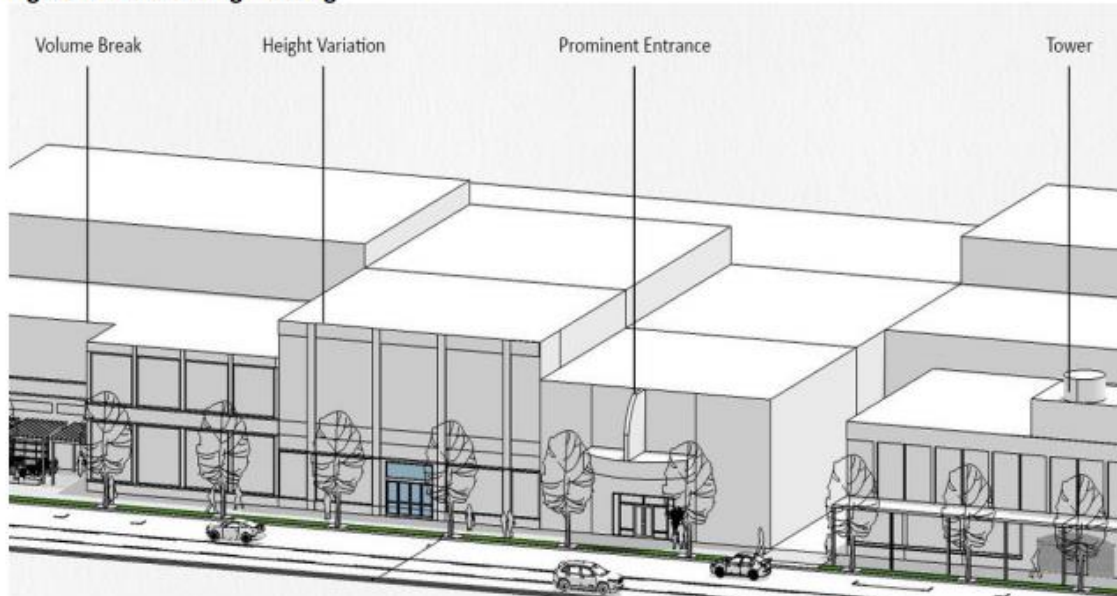


- 1 maximum of 15 feet above eave line.
- 2 b. Trellises may extend above the maximum height up to 8 feet.
- 3 F. *Floor height.*
- 4 1. Floor height shall be measured as provided in the Florida Building Code.
- 5 2. Parking garages are exempt from the minimum floor height requirements.

#### Section 30-4.14. Building Design Standards.

- A. *Building massing.* Large building volumes shall be divided to appear as smaller volumes grouped together. Volume breaks may be achieved by volume projections and recesses, and varying heights and roof lines. Therefore, building facades shall not exceed 60 feet along a street frontage without providing a substantial volume break such as a volume projection or recess, a tower or bay, or an architecturally prominent public entrance. The recesses and projections shall have a minimum depth and width of 10 feet.

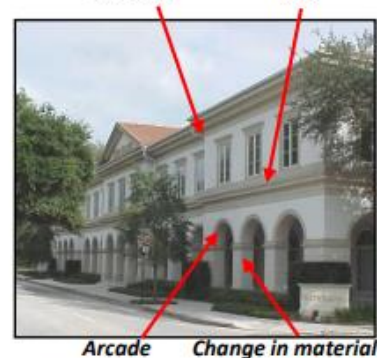
**Figure V - 16: Building Massing**



- B. *Facade articulation.* The standards contained in this section apply to multi-family, nonresidential and mixed-use buildings. Building facades along streets shall maintain a pedestrian scale by integrating the following architectural elements:

1. Facades shall not exceed 20 horizontal feet without including at least one of the following elements:
  - a. A window or door.
  - b. Awning, canopy or marquee.
  - c. An offset, column, reveal, void, projecting rib, band, cornice, or similar element with a minimum depth of six inches.

**Figure V - 18: Façade Elements**

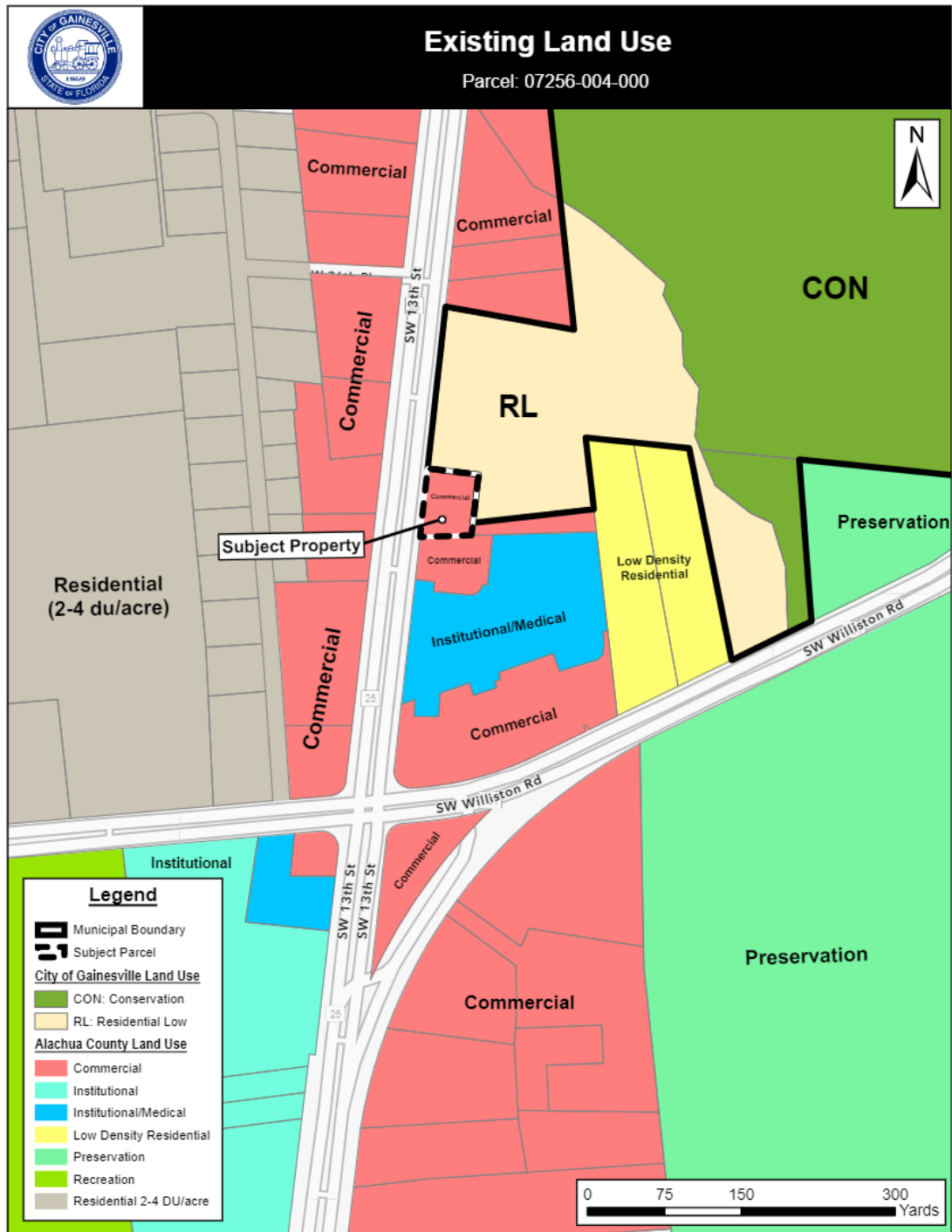






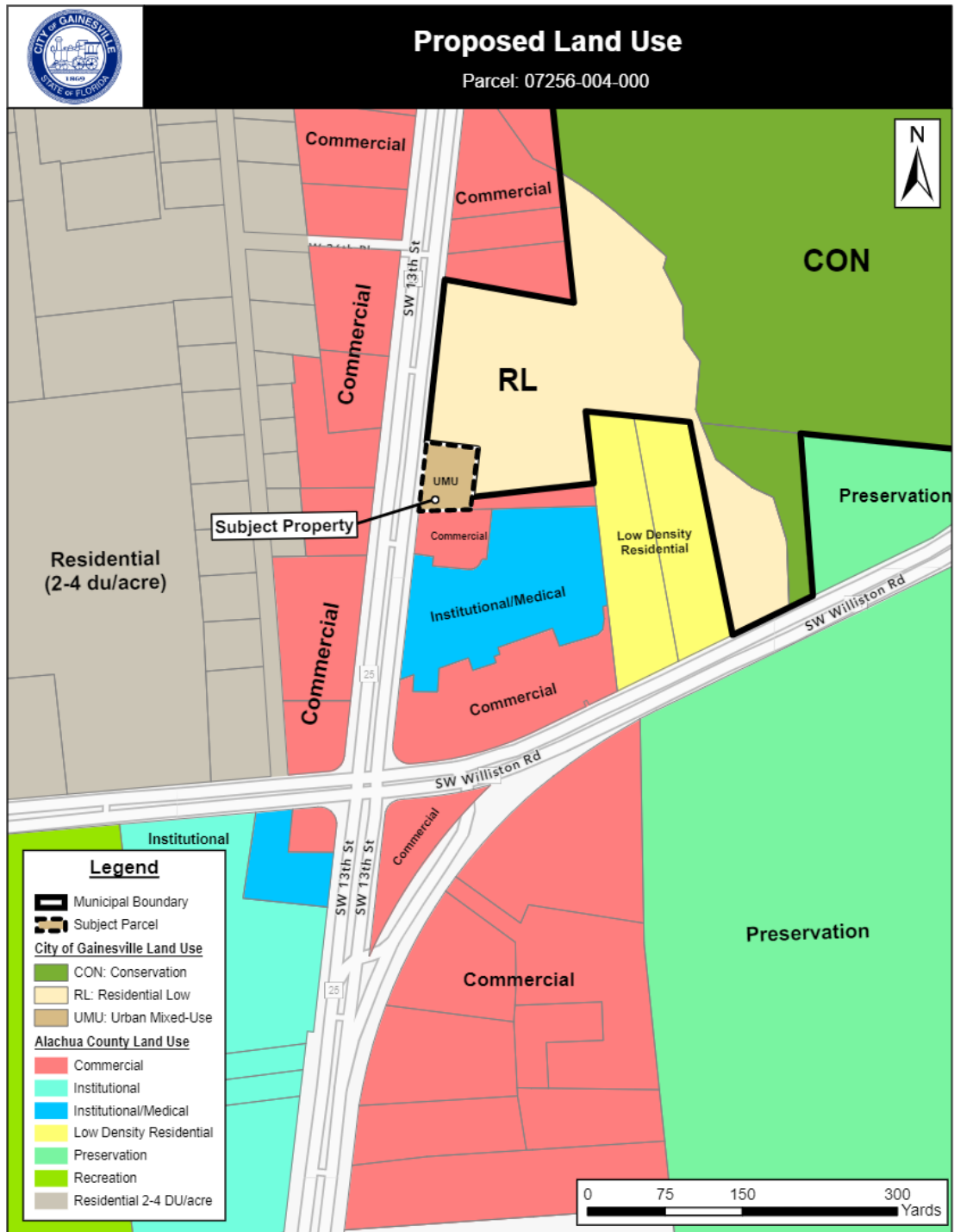
## **APPENDIX C: Maps**

Map:1 Existing Land Use: Alachua County Commercial



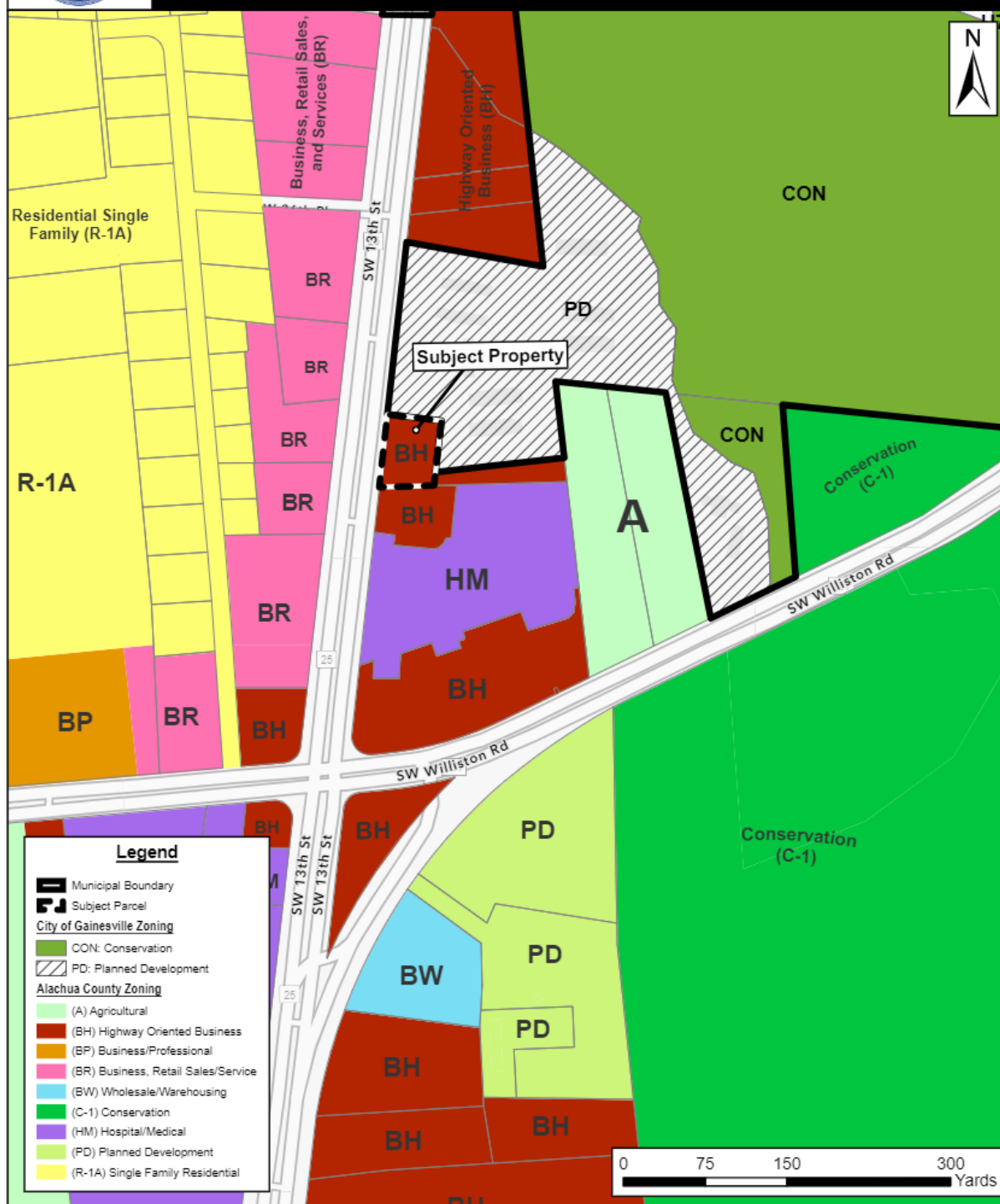


Map 2: Proposed Land Use: Urban Mixed Use (UMU)

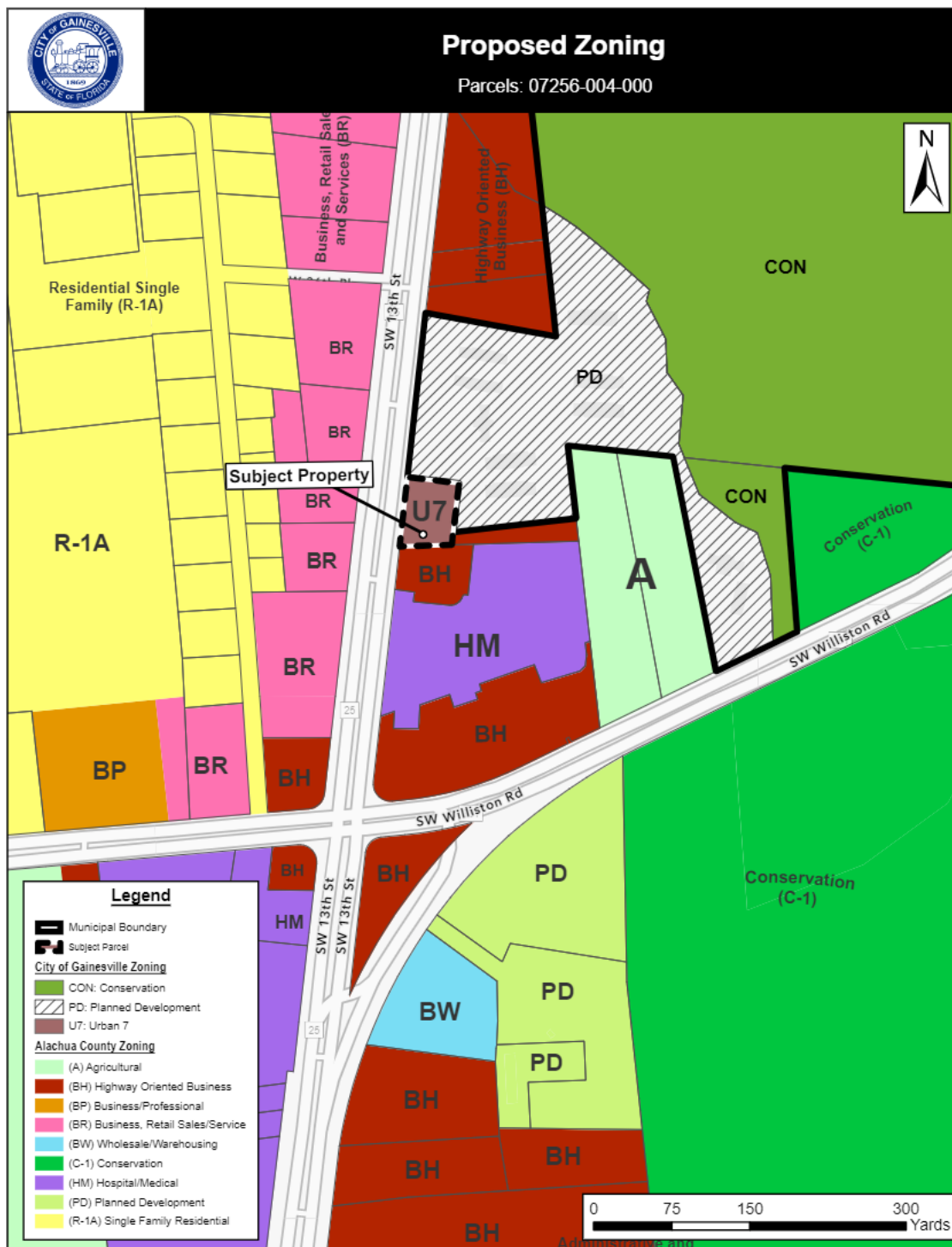


## Existing Zoning

Parcel: 07256-004-000



Map 4: Proposed Zoning: Urban Transect Zone U7



## **APPENDIX D: Notification**









